

Chapter amendments approved:	OMPC	Owensboro	Daviess Co.	Whitesville
Re-adoption of Public Improvement Specifications	24-Mar-77	01-Apr-77	20-Apr-77	?
Revised Public Improvement Specifications	18-Apr-81	22-May-81	26-May-81	06-Jul-81
2002 Revised Public Improvement Specifications	08-Aug-02	No action required by legislative bodies		
Revisions to Exhibit 9-1	10-03-11	No action required by legislative bodies		

9.0 PURPOSE. The purpose of this chapter is to outline the requirements for accomplishing the proper removal and replacement of existing facilities necessitated by construction activity.

9.1 CONCRETE SIDEWALKS. Where concrete sidewalks are cut or disturbed during any construction activity, they shall be replaced in full, in accordance with Chapter 4 of these specifications.

In general, concrete sidewalk shall be tunneled under when encountered in trenching for pipe. In case rock excavation is encountered, or it is found necessary by the Engineer, then sidewalks shall be cut and not tunneled for the pipe. When concrete sidewalks are tunneled under, they shall be backfilled by mechanically tamping earth under the portion undermined so as to prevent settlement.

When replacing concrete sidewalks, sidewalks shall be removed to the nearest tooled or expansion joint, which is at least six (6) inches beyond either edge of the trench. The cut edges shall be cleaned and kept moist during pouring to insure a good bond with new concrete.

After the trench has been backfilled, a base course of three (3)-inch thick DGA shall be placed and tamped. Immediately prior to pouring the concrete, the DGA base shall be thoroughly wetted.

The sidewalk shall consist of four (4) inches of Class "A" concrete struck off to accurately placed screeds and worked with a wooden float until the mortar appears on the top. After the surface has been thoroughly floated, it shall be brushed to leave markings of a uniform type similar to the existing walk. All joints and edges shall be finished with an edging tool. The allowable variation shall be 1/8 inch in 10 feet transversely and longitudinally.

9.2 CONCRETE PAVEMENT. Where Portland Cement concrete streets and driveways are removed, they shall be reconstructed to the original lines and grades and in accordance with Chapter 3 "Streets" of these

specifications, and Exhibit 9-1, "Permanent Street Cut Repair Detail Sheet."

The existing concrete pavement shall be removed to the nearest construction joint. Partial removal of concrete streets shall only be allowed if a drill and dowel method is approved by the Engineer.

Concrete pavement replacement shall be in accordance with Chapter 3 "Streets" of these specifications.

Concrete pavement shall be a minimum of six (6) inches thick of Class "P" Concrete. It shall be placed, worked and finished according to the requirements for the previous section on concrete pavements, and protected until concrete reaches specified strengths of 3,500 psi.

9.3 BITUMINOUS HIGHWAY, STREET AND DRIVEWAY REPLACEMENT. The Contractor shall replace those sections of existing highways, streets and driveways that have been removed during construction. He shall reconstruct same to the original lines and grades and in such manner as to leave all such surfaces to the original lines and grades in accordance with Chapter 3 "Streets" of these specifications, and Exhibit 9-1, "Permanent Street Cut Repair Detail Sheet."

Prior to trenching, the pavement shall be scored or cut to straight edges at least 12 inches outside each edge of proposed trench to avoid unnecessary damage to the remainder of the paving. Edges of the existing pavement shall be recut and trimmed to square, straight edges after the pipeline has been installed and prior to placing the new base and pavement. In the event that pavement edges are not damaged, recutting shall not be required.

Backfilling the trench shall be in accordance with the previous section on backfilling trenches. Base course for the paving shall be Dense Graded Aggregate and shall be placed in accordance with Chapter 3 "Streets", Section 3.5 to a depth equal to the existing base course, but not less than six (6) inches in highways and six (6) inches in driveways within the public right-of-way.

The wearing surface of the streets and driveways shall be plant-mix bituminous concrete, Class I, furnished and placed in accordance with the current requirements of the KTC Specifications to a depth equal to the existing pavement, but not less than two (2) inches in the highways and streets and 1-1/2 inches in driveways.

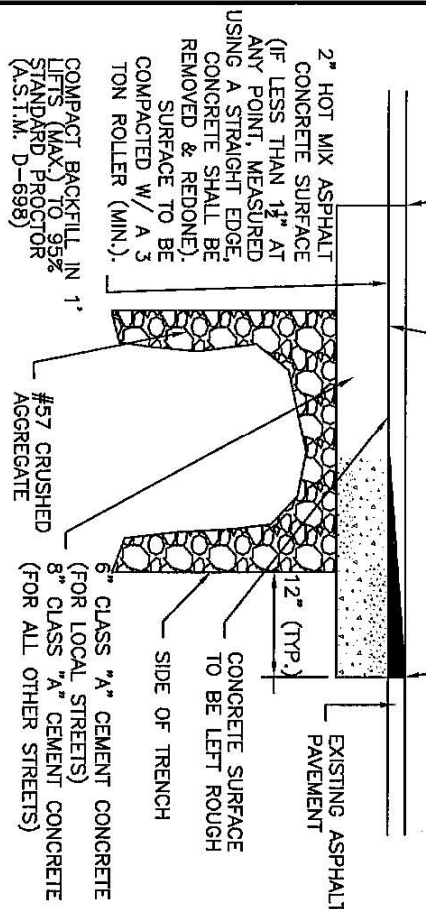
9.4 REMOVING AND REPLACING CONCRETE CURB AND GUTTER. When concrete curb and gutter is cut or disturbed during the construction work it shall be removed in its entirety to the nearest construction joint and replaced using Class "A" concrete, in accordance with Chapter 4 "Sidewalks, Curb and Gutter" of these specifications. The replacement shall be in accordance with the previous section on construction of concrete curb and gutter.

9.5 REMOVING AND REPLACING CONCRETE DRIVEWAY APRONS.

When curbs are removed to receive driveways, the curb may be re-poured monolithic with driveway apron, provided the one-inch valley through the curb section is replaced for positive drainage. A saw joint should be cut at the back of curb. When the sidewalk continues across the driveway apron, the sidewalk pattern shall also be continued across the apron. Concrete driveway aprons shall be a minimum of six (6) inches thick Class "A" concrete. Concrete shall be placed, worked, and finished according to the requirements of Chapter 13 "Concrete Specifications Materials & Methods".

9.6 REPLACEMENT OF EXISTING MAIL BOXES, CULVERTS, FENCES AND OTHER SUCH FACILITIES. Existing mail boxes, drainage culverts, fences and the like shall not be damaged or disturbed unless necessary, in which case, they shall be replaced in as good condition as found as quickly as possible. Existing materials shall be re-used in replacing such facilities when materials have not been damaged by the Contractor's operations. Existing facilities damaged by the Contractor's operation shall be replaced with new materials of the same type at the Contractor's expense.

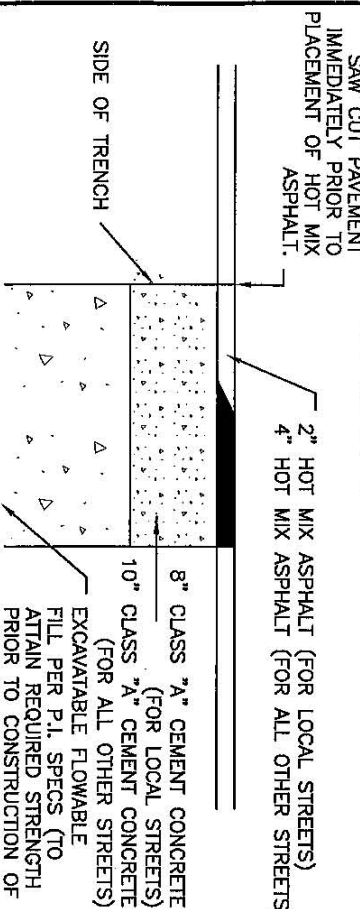
ASPHALT STREET CUT
ALTERNATE "A"
 TACK CONCRETE SURFACES AS PER K.I.C. STANDARDS



NOTE: FOR WIDTH OF TRENCH OVER 4\"/>

CONTRACTOR SHALL PROVIDE ADEQUATE DETOURING OR PLATE OVER CONCRETE UNTIL 3,500 P.S.I. COMPRESSIVE STRENGTH HAS BEEN ATTAINED. HIGH EARLY STRENGTH CONCRETE REQUIRED UNLESS OTHERWISE SPECIFIED BY THE CITY ENGINEER.

ASPHALT STREET CUT
ALTERNATE "B"

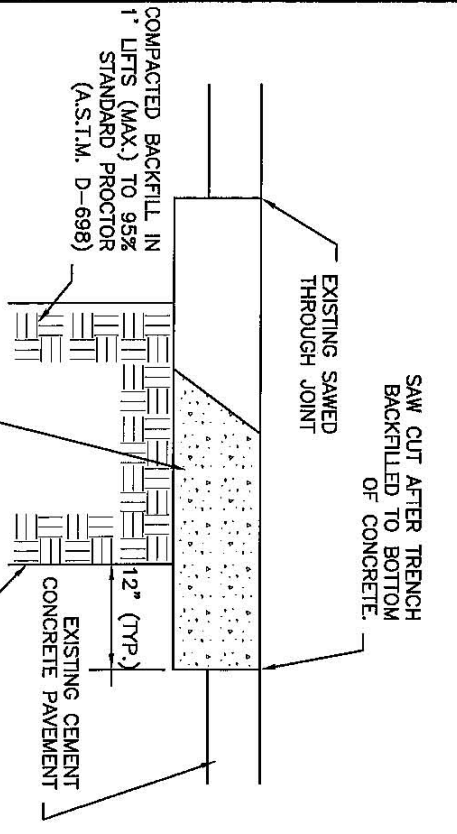


GENERAL NOTES FOR ALL STREET CUT REPAIR
 "NO CUT STREETS SHALL BE CONSIDERED ON AN INDIVIDUAL BASIS.
 NON-CONFORMANCE TO STANDARDS SHALL RESULT IN THE CONTRACTOR REMOVING AND REPLACING THE STREET CUT."

CONTRACTOR SHALL PROVIDE ADEQUATE DETOURING OR PLATE OVER CONCRETE SLAB UNTIL REQUIRED COMPRESSIVE STRENGTH HAS BEEN ATTAINED.

NOTE: 2" HOT MIX ASPHALT SHALL BE PLACED AFTER CONCRETE SLAB HAS ATTAINED SUFFICIENT STRENGTH BUT PRIOR TO THE OPENING OF THE STREET CUT TO TRAFFIC

CEMENT CONCRETE STREET CUT



8" CLASS "A" CEMENT CONCRETE (FOR LOCAL STREETS)
 10" CLASS "A" CEMENT CONCRETE (FOR ALL OTHER STREETS)

NOTE: FOR WIDTH OF TRENCH OVER 4\"/>

MINIMUM DISTANCE FROM EDGE OF SAW CUT TO EXISTING CONCRETE JOINT SHALL BE 3'-0". INTERNAL 90° ANGLES WILL NOT BE ALLOWED.
 IF MORE THAN 50% OF THE PAVEMENT SLAB IS TO BE CUT, THE CONTRACTOR SHALL REMOVE AND REPLACE THE ENTIRE PAVEMENT SLAB.

CONCRETE DESIGN JOINTS TO BE RE-ESTABLISHED WITH LOAD TRANSFER

SHEET REVISED 01_10_2011

OWENSBORO METROPOLITAN
 PUBLIC IMPROVEMENT SPECIFICATIONS

CHAPTER 9
 EXISTING FACILITIES:
 REMOVAL & REPLACEMENT

PERMANENT STREET CUT REPAIR
 DETAIL SHEET

EXHIBIT NO. 9-1
 NOT TO SCALE